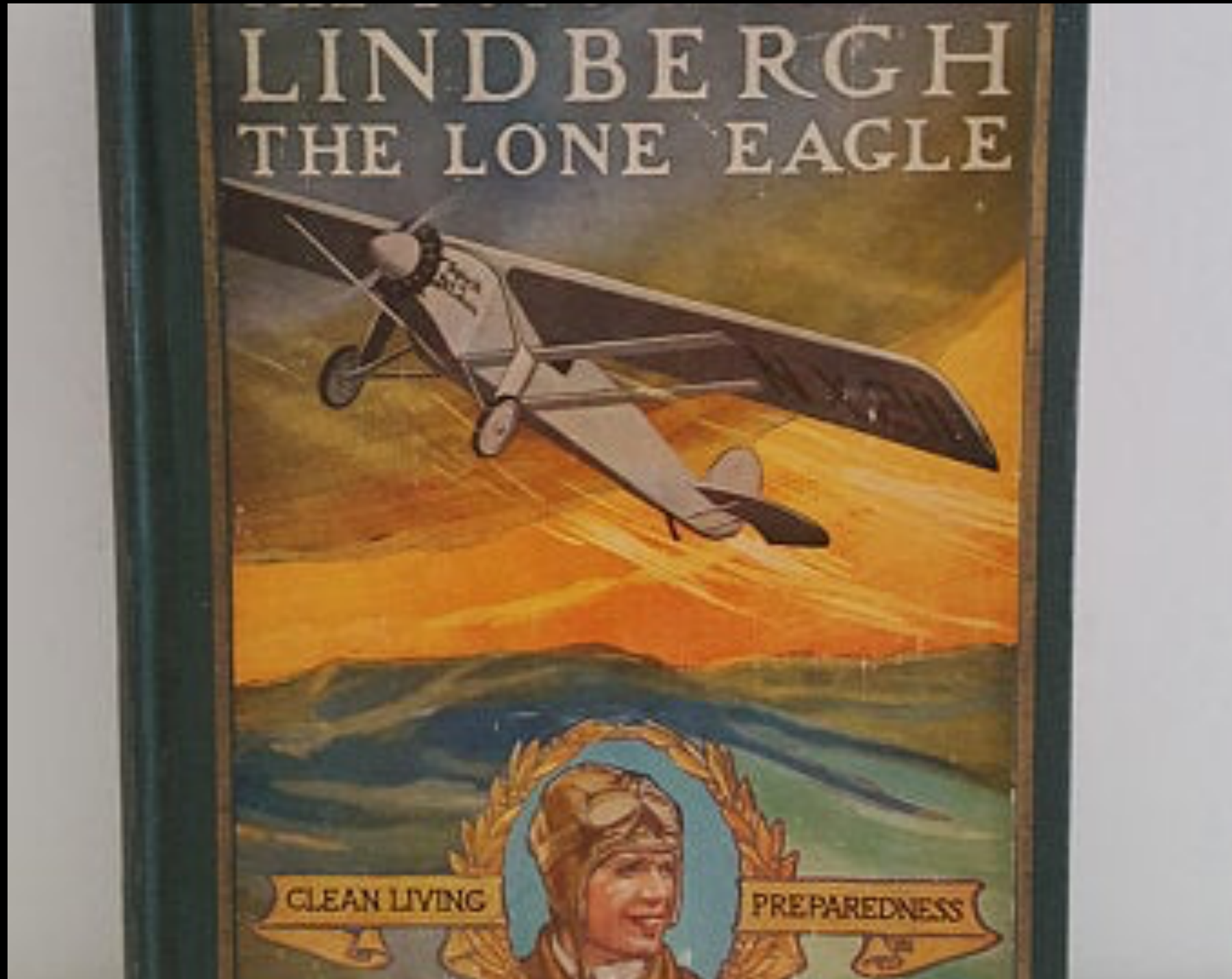
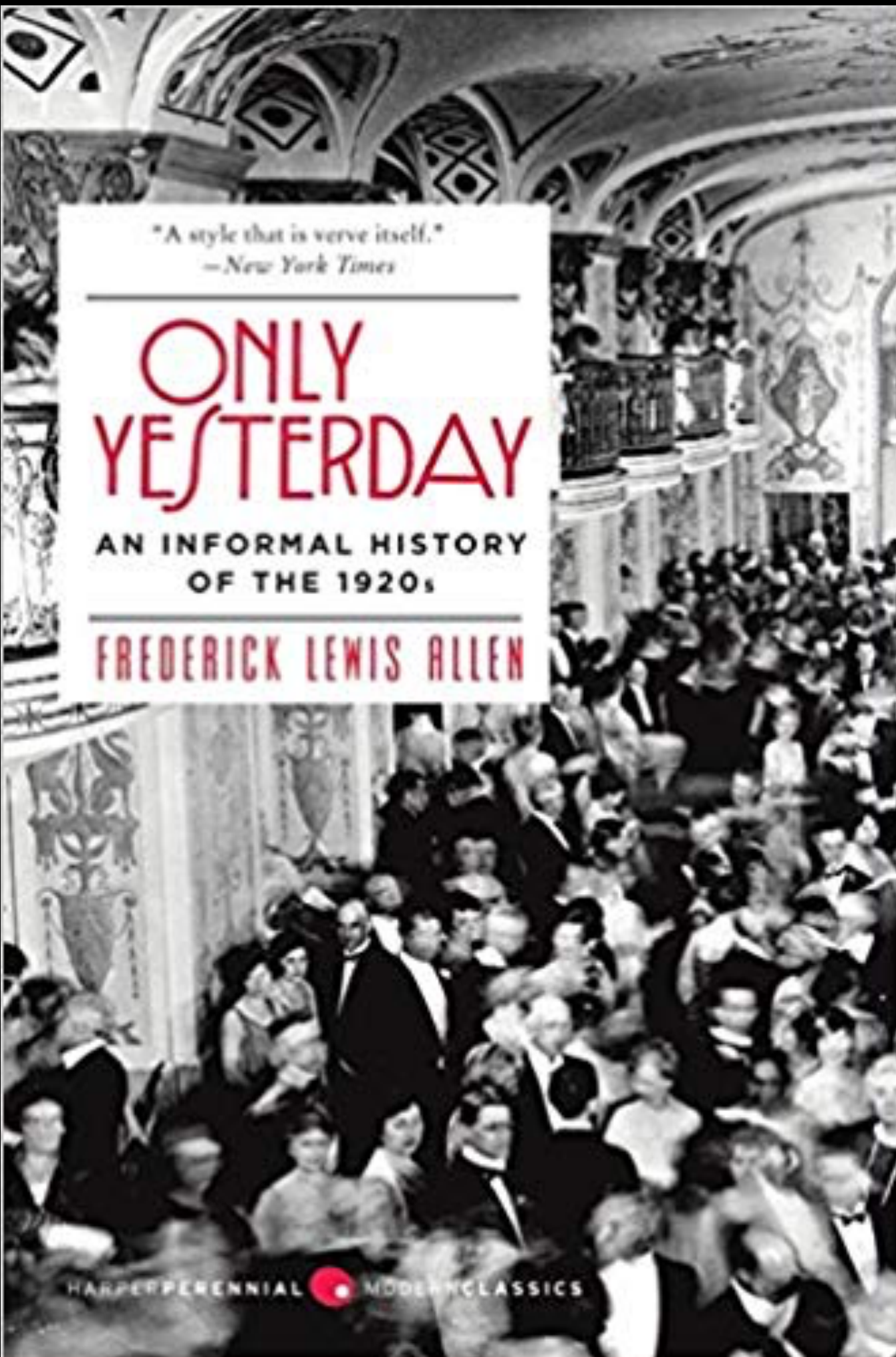


INSTITUTE FOR THE STUDY OF WESTERN CIVILIZATION
THE ROARING TWENTIES
LECTURE 3: THE RELUCTANT GIANT
AND THE LONE EAGLE







"A style that is verve itself."

—New York Times

ONLY YESTERDAY

AN INFORMAL HISTORY
OF THE 1920s

FREDERICK LEWIS ALLEN

HARPERPERENNIAL MODERNCCLASSICS



THE NUMBER ONE BESTSELLER

BILL BRYSON

ONE SUMMER

AMERICA

1927

'Sublime'
Observer

'Fascinating'
Sunday Times

'Gripping'
The Times



THE SPECTACLE OF FLIGHT

AVIATION AND THE WESTERN IMAGINATION: 1920-1950

ROBERT WOHL

HOLLYWOOD



The PIONEERS



KEVIN BROWNLOW
JOHN KOBAL









WESTERN
HEMISPHERE

—AND THEY ARE
THE LEADING MEMBERS
OF THE
LEAGUE OF NATIONS
TO PROMOTE
WORLD PEACE!!

EASTERN
HEMISPHERE

SEEDS
OF
FUTURE
WARS

SEEDS
OF
FUTURE
WARS

SEEDS
OF
FUTURE
WARS

SEEDS
OF
FUTURE
WARS

VLADIVOSTOK
SIBERIA

ADRIATIC COAST

FIUME

TYROL

TURKEY

PERSIA

MESOPOTAMIA

ASIA MINOR

SYRIA

RHINE CITIES

SAAR BASIN

W. G. F. 1919

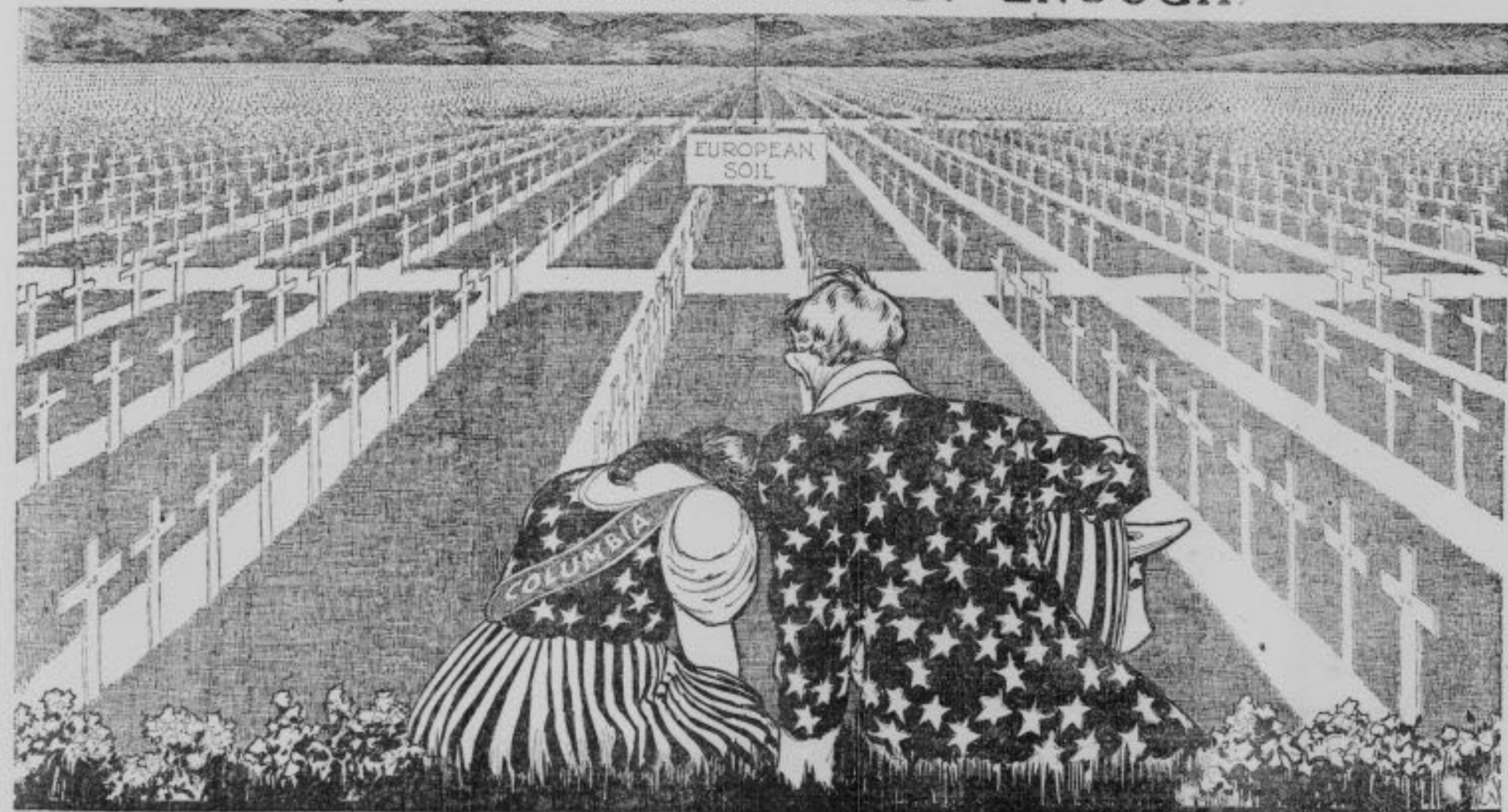
EDITORIAL SECTION

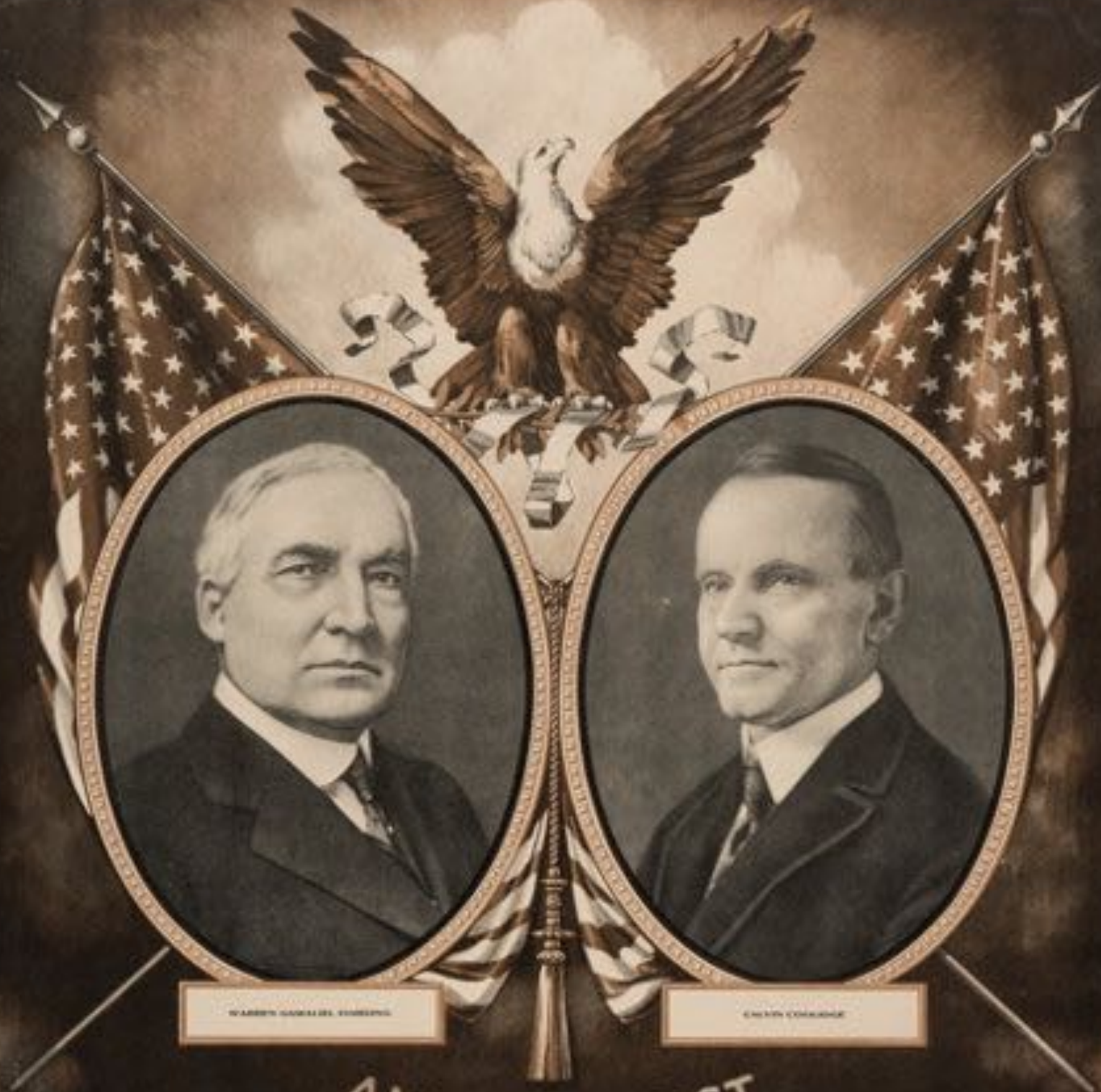
DRAMATIC CITY LIFE
AND SOCIETY

New York American

SUNDAY, OCTOBER 10, 1920

35,000 AMERICAN DEAD. ENOUGH!





WARREN G. HARDING

CHARLES McNAMARA

AMERICA FIRST

For
PRESIDENT

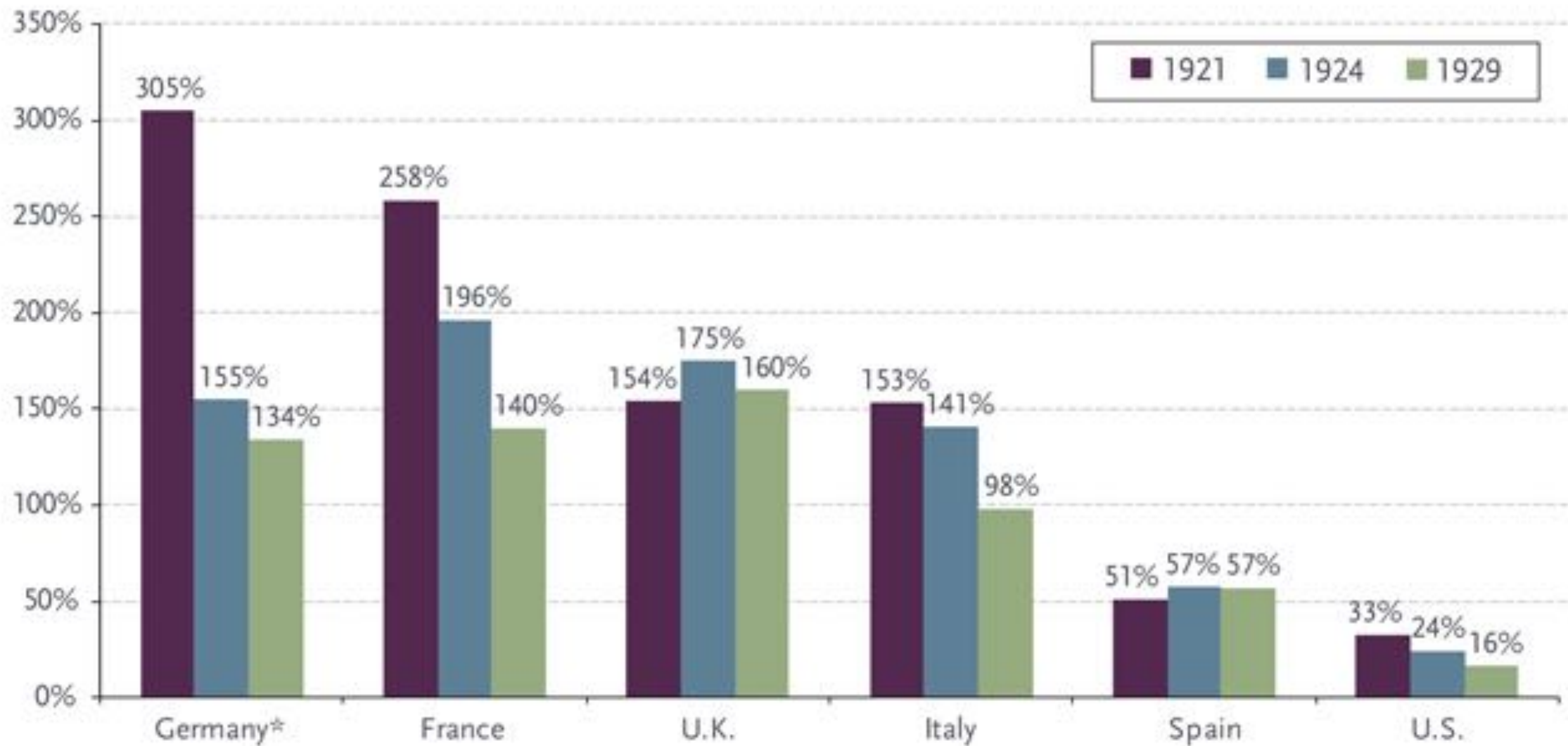
WARREN G. HARDING
 Former U.S. Senator from Ohio, elected in 1920, served as 29th President of the United States. He was a member of the Republican Party and a member of the National Association of Manufacturers. He was a member of the National Association of Manufacturers and a member of the National Association of Manufacturers. He was a member of the National Association of Manufacturers and a member of the National Association of Manufacturers.

For
VICE PRESIDENT

CHARLES McNAMARA
 Former U.S. Senator from Ohio, elected in 1920, served as 29th Vice President of the United States. He was a member of the Republican Party and a member of the National Association of Manufacturers. He was a member of the National Association of Manufacturers and a member of the National Association of Manufacturers.

EUROPEAN DEBT AFTER THE GREAT WAR

In the years following World War I, European nations were buried in debt. Germany, slapped with punitive war reparations, routinely defaulted on its payments even after several restructuring efforts were attempted in 1920s.



Source: Reinhart and Rogoff, Guggenheim Investments. *Note: German debt to GDP ratio includes reparation obligations.



Sacco and Vanzetti

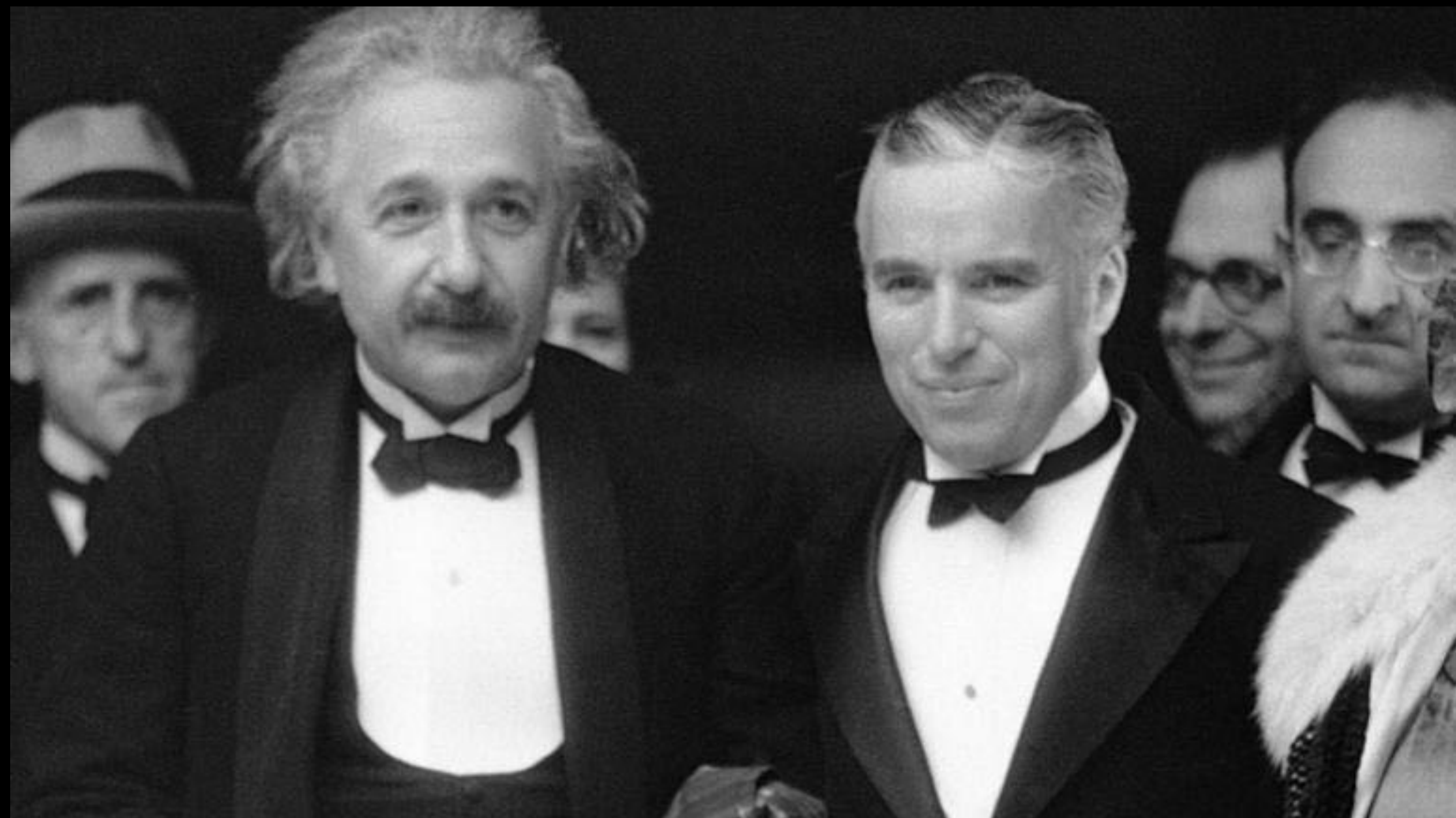


*Conférence
de la Paix*

H. Manuel
Phot.

J.N.
PARIS
783

ANDRÉ TARDIEU
Haut Commissaire des Affaires de Guerre Franco-Américaines
Délégué



















Frederick Lewis Allen



"All the News That's
Fit to Print."

The New York Times.

THE WEATHER

Weather, Temperature, Wind, Rain, Snow, etc.
For the United States and Possessions

VOL. LXXV. NO. 34,881

NEW YORK, MONDAY, JUNE 24, 1930. TWENTY-THIRD YEAR.

TWO CENTS

ALCOCK AND BROWN FLY ACROSS ATLANTIC; MAKE 1,980 MILES IN 16 HOURS, 12 MINUTES; SOMETIMES UPSIDE DOWN IN DENSE, ICY FOG

OUR TROOPS CROSS BORDER TO STOP JUAREZ FIGHTING

Infantry, Cavalry, and Artillery
Enter Mexico and Are Engag-
ing the Villistas.

BOMB ALREADY IN ACTION

General Crowl States That He
Purposes Only to Protect City
and Place No Intervention.

SHOTS HIT AMERICANS

Two Killed, Several Wounded, When
Villistas Fired at Americans at
Juarez.

At Juarez, Texas, Monday night, the
two sides, American and Mexican,
were engaged in a battle which
lasted all night long.

The two sides, American and Mexican,
were engaged in a battle which
lasted all night long.

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were engaged in a battle which
lasted all night long.

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were engaged in a battle which
lasted all night long.

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were engaged in a battle which
lasted all night long.

President Wilson to Sell for Bond About June 24

President Wilson is expected to sell
his bond about June 24. The bond
will be sold at a price of 100 cents
per \$100 of face value. The bond
will be sold at a price of 100 cents
per \$100 of face value.

GERMANS OBJECTED STRONGLY TO TERMS

Reply to Allies Stated They
Thought Themselves Equal
Negotiators.

PROTESTS AVOIDED LITTLE

Final Answer from Council
Gives Relief on Only a
Few Points.

By BRUNO H. HENNING

The Council of Four, which met
in Paris, has just issued its
final answer to the German
demands. The answer is a
relief on only a few points.



Captain John Alcock



Lieutenant Arthur W. Brown

Airmen Who Accomplished the First Non-Stop Transatlantic Flight

FOCH OFF TO RHINE;
PERSHING TO GO, TOO

Northcliffe, Congratulating Foch,
Says That Airplane Is

FRENCH REDS RIOT;
KILLED 47 HUNG

LAND AT CLIFDEN, IRELAND, IN A BOG

Airmen Moving at an 120-Mile Speed
Were Unable to See Sun, Moon, or
Stars for Hours Owing to Fog.

THEY LOOPED THE LOOP UNINTENTIONALLY AT TIMES

Fliers Temporarily Dazed and Deafened by Land-
ing—Alcock Expects to Continue Journey to Eng-
land—London Unprepared for Quick Flight.

Captain Alcock's Own Narrative Of His Flight From Newfoundland to Ireland

By Captain J. Alcock, R. N. E.
I have never in my life had a more
eventful journey. The weather was
terrible, but we were determined to
make it. We had a very good
time. We were very lucky. We
were very happy. We were very
satisfied. We were very proud.





Raymond Orteig (1870-1939)



**Rene Fonck and his
Sikorsky**





Navy pilots Noel Davis and Stanton Wooster



1926

RICHARD E. BYRD AND FLOYD BENNETT
HAVE FLOWN OVER THE NORTH POLE
(DISCOVERY OF BYRD'S DIARY APPEARS TO
CAST SOME DOUBT ON THE CLAIM.)



R. WANAMAKER

5567-1

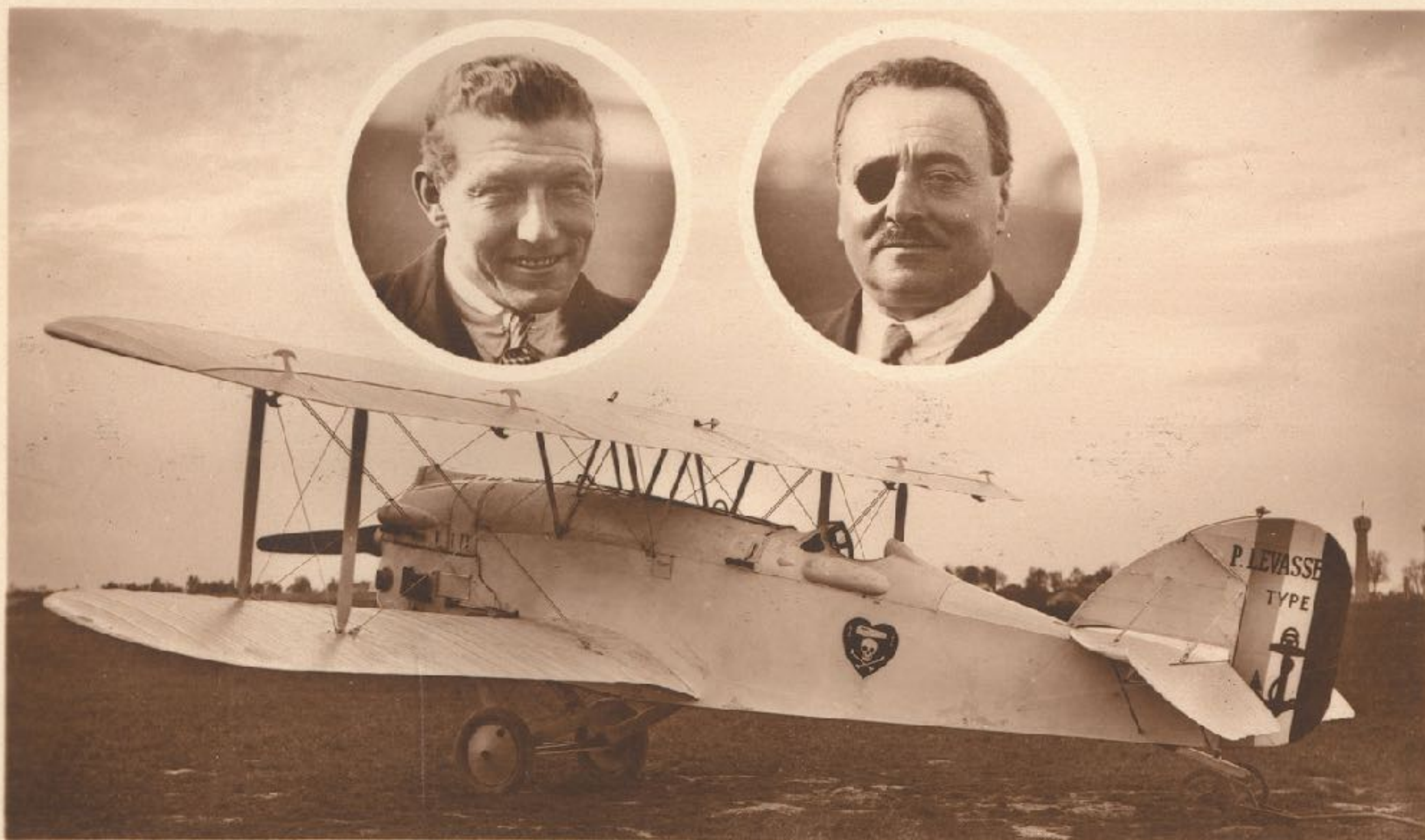




Charles Levine



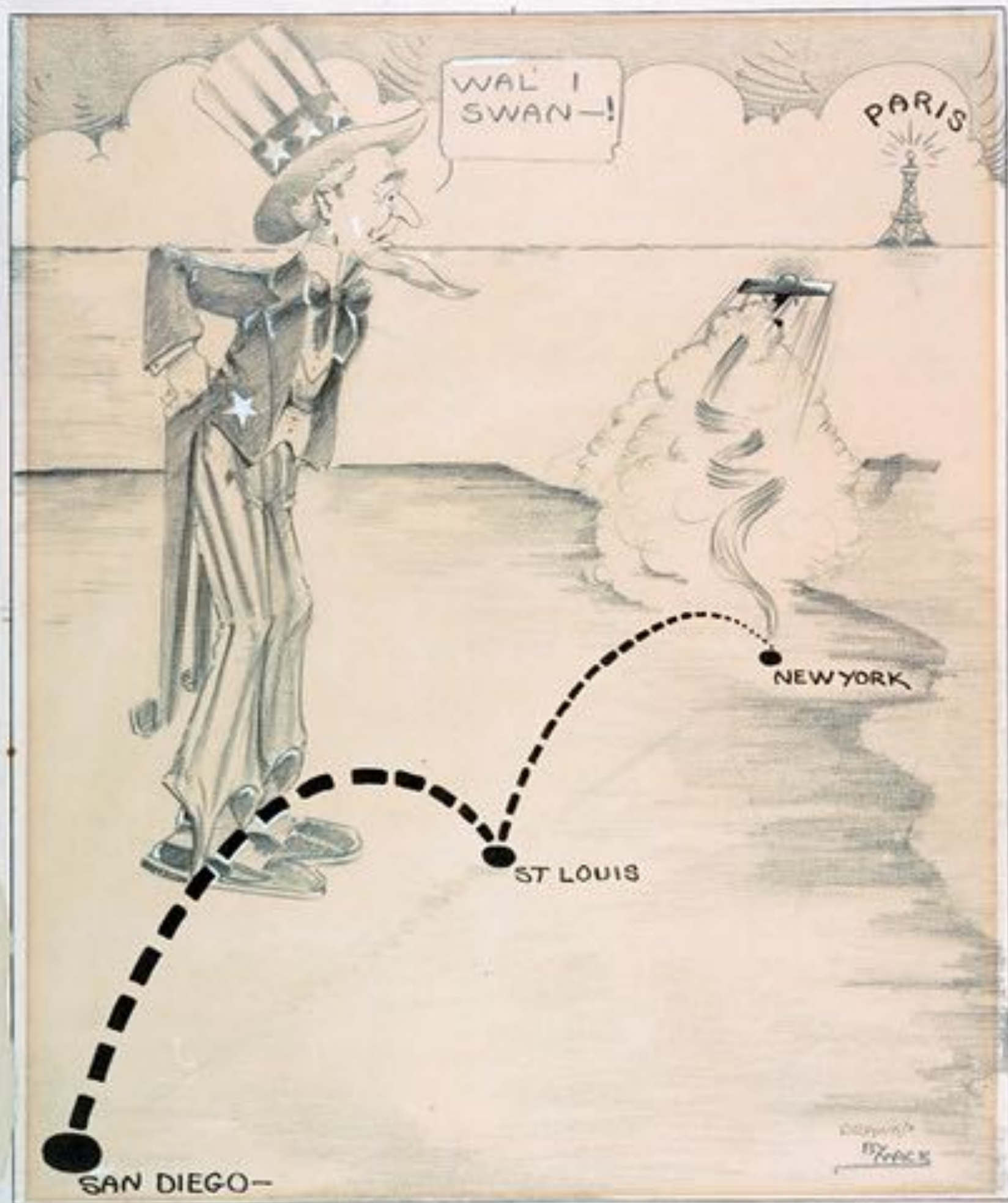
The Bellanca

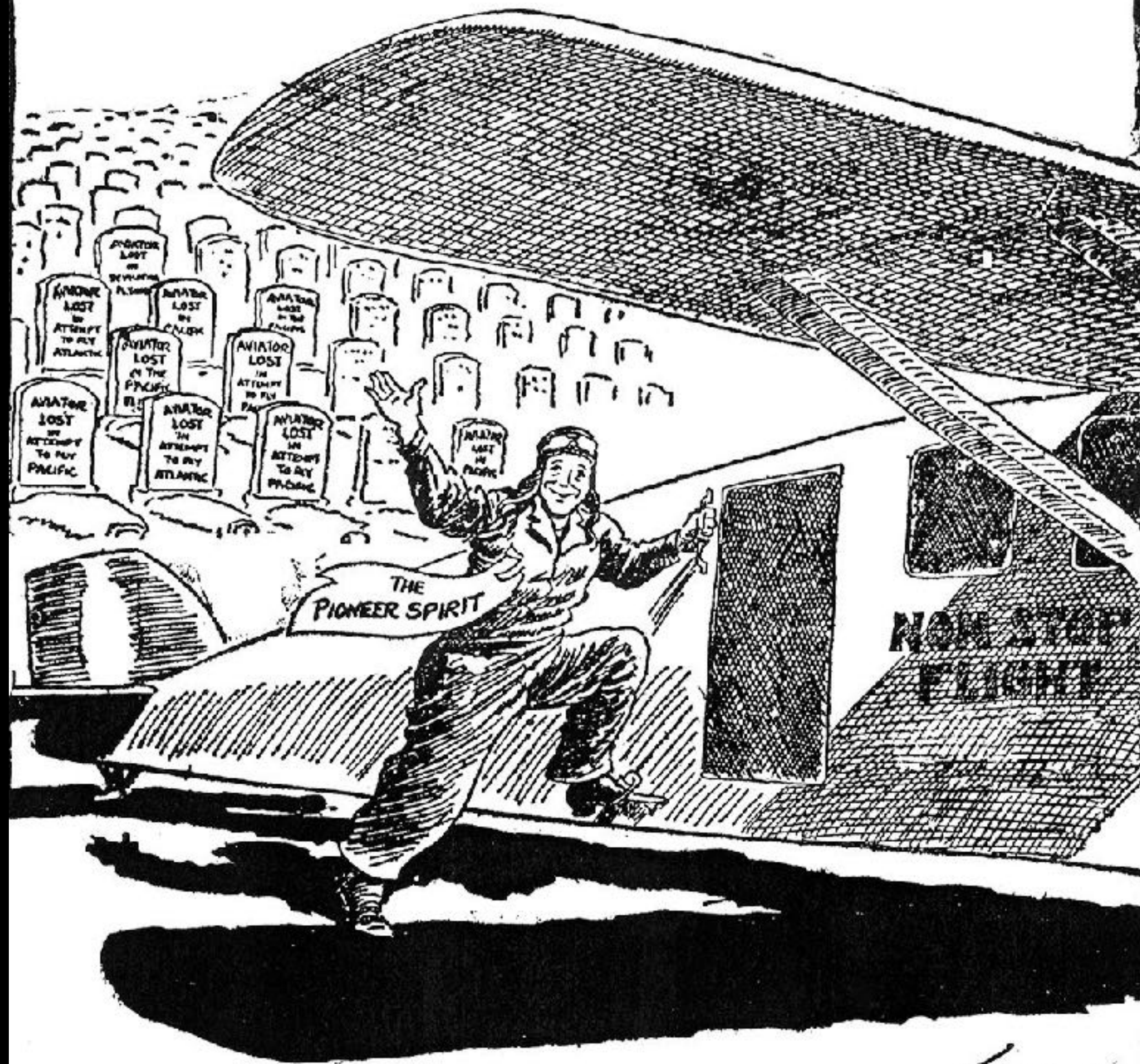







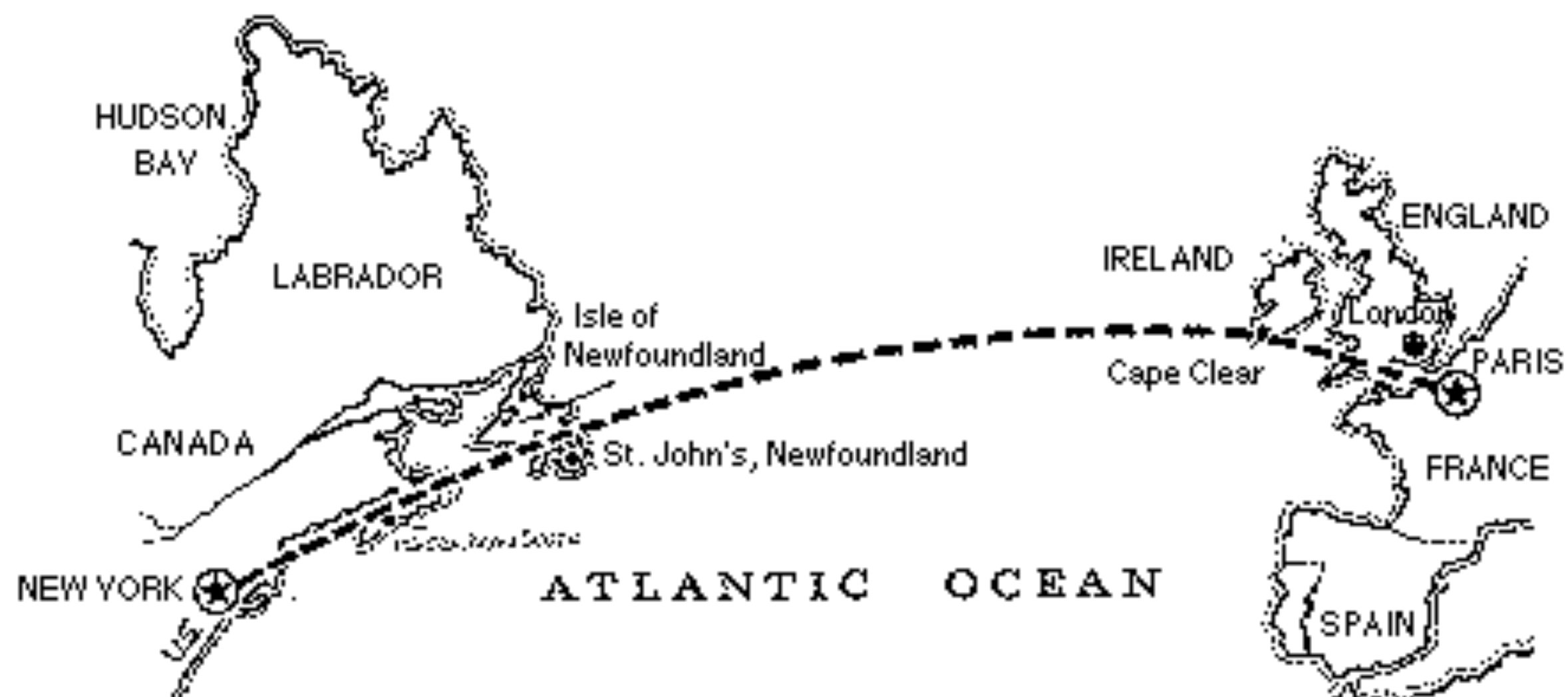
NUNGESSER et COLI — Les Héros du raid "PARIS-NEW YORK"
et l'avion marin P. Levasseur, moteur Lorraine-Diétrich.



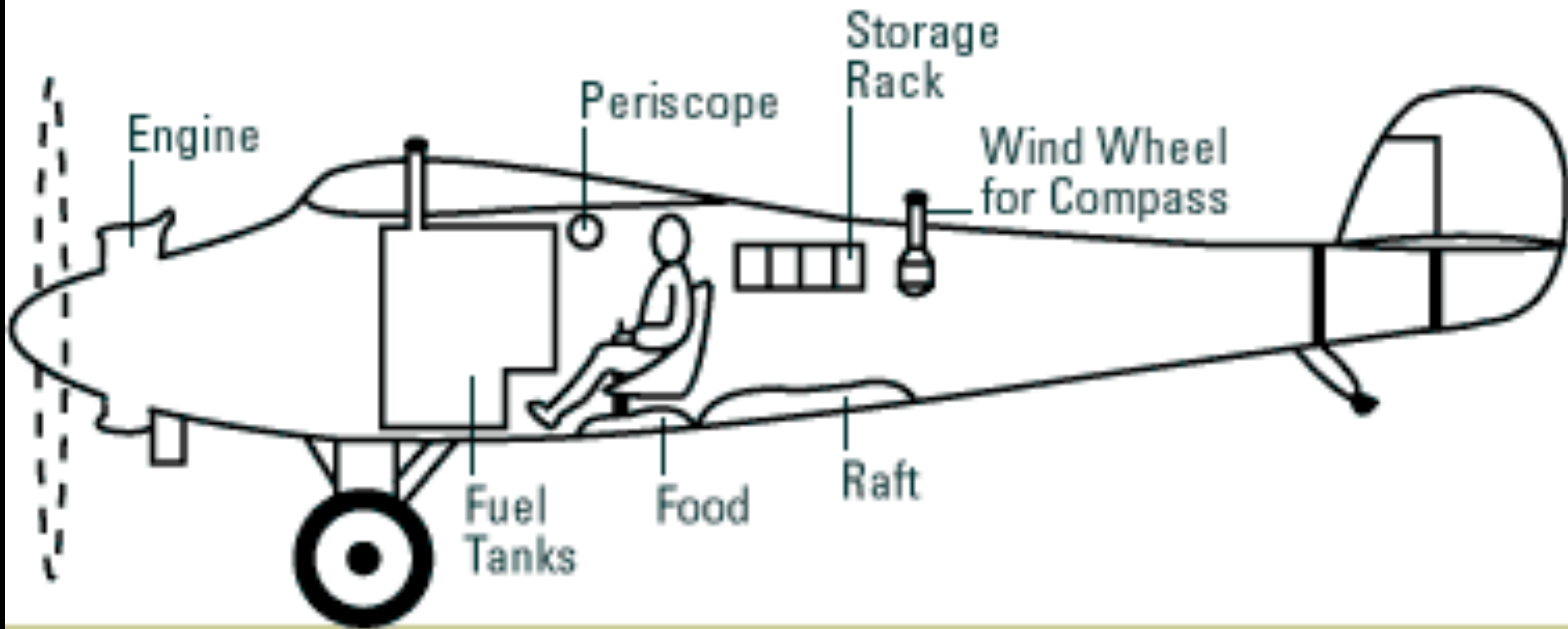


This is a black and white aerial photograph of a rural landscape. In the foreground, there is a large, flat, open field. A red arrow points from the text 'Lindbergh's Takeoff May 20, 1927' to a specific spot in this field. The middle ground shows a line of trees and some small buildings. In the background, there are more fields and a distant town or city. The overall scene is a typical rural landscape from the early 20th century.

Lindbergh's
Takeoff
May 20, 1927



The Spirit of St. Louis





LINDBERGH DOES IT! TO PARIS IN 33½ HOURS; FLIES 1,000 MILES THROUGH SNOW AND SLEET; CHEERING FRENCH CARRY HIM OFF FIELD

COULD HAVE DONE 500 MILES FURTHER

Gasoline for at Least That Much More—
Flew at Times from 10 Feet to
10,000 Feet Above Water.

ATE ONLY ONE AND A HALF OF HIS FIVE SANDWICHES

Fell Asleep at Times but Quickly Awoke—Clipped
of His Adventure in Brief Interview
at the Embassy.



MAP OF LINDBERGH'S TRANSCONTINENTAL FLIGHT, SHOWING THE SPEED OF HIS TRIP.

CROWD ROARS THUNDEROUS WELCOME

Breaks Through Lines of Soldiers and
Police and Surging to Plane Lifts
Weary Flier from His Cockpit

ARMY'S SAFE HIM FROM FRENCH MOB OF 10,000

Paris Boulevards Ring With Celebration After Day
and Night Watch—American Flag Is Carried
Far and Wildly Acclaimed.

LINDBERGH'S OWN STORY TONIGHT

Captain Charles A. Lindbergh was interviewed after his arrival in Paris last night by the press, at the embassy, in connection with his flight. After he had eaten, he will answer the first one of the questions asked by the press at Lindbergh's New York Press.

By CHARLES LINDBERGH

When I was in the air, I was very tired, but I was very happy.

PARIS, Sunday, May 20.—Captain Lindbergh was the second to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight. He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

A staff of American doctors who had arrived at the airport to meet him, and a staff of French doctors who had arrived at the airport to meet him, and a staff of French doctors who had arrived at the airport to meet him.

"I'm here to have a bath and a glass of milk. I want to be comfortable," Lindbergh replied when the doctor asked him what he would like to have.

A bath was given immediately and in less than five minutes the weary flier was in bed. He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

"No War Warning," He Tells Story

"There is no war warning about me," the captain said. "I'm here to have a bath and a glass of milk. I want to be comfortable," Lindbergh replied when the doctor asked him what he would like to have.

It was reported that the young man was the first of his generation to have done so, and to do so in the first and last of his life.

LEVINE HANGING BELLANCA FLIGHT

Levine was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

ARMY'S SAFE HIM FROM FRENCH MOB OF 10,000

Lindbergh was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

Though it was reported that he was very tired, he was very happy. He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

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CAPTAIN CHARLES A. LINDBERGH,
who flew alone across the Atlantic, had just a bath
in the hotel at the airport.

New York Stages Big Celebration

LINDBERGH TRIUMPH THRILLS COULIDGE

President Calhoun spoke to "Honor War" and Congress to Congress and War.

CAPITAL, TOWNERS WITH AIR

Lindbergh was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

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It was reported that the young man was the first of his generation to have done so, and to do so in the first and last of his life.

By HENRY L. GARDNER

PARIS, Sunday, May 20.—Lindbergh's flight to Paris was the first of its kind, and it was the first of its kind.

Paris was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

Though it was reported that he was very tired, he was very happy. He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

Lifted From His Cockpit

He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

French Mob of 10,000

Lindbergh was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.

Breaks Through Lines of Soldiers and Police

He was the first to be interviewed at the embassy after his arrival in a pair of dark-colored trousers and a white shirt, the top of a hat and a pair of his flight.



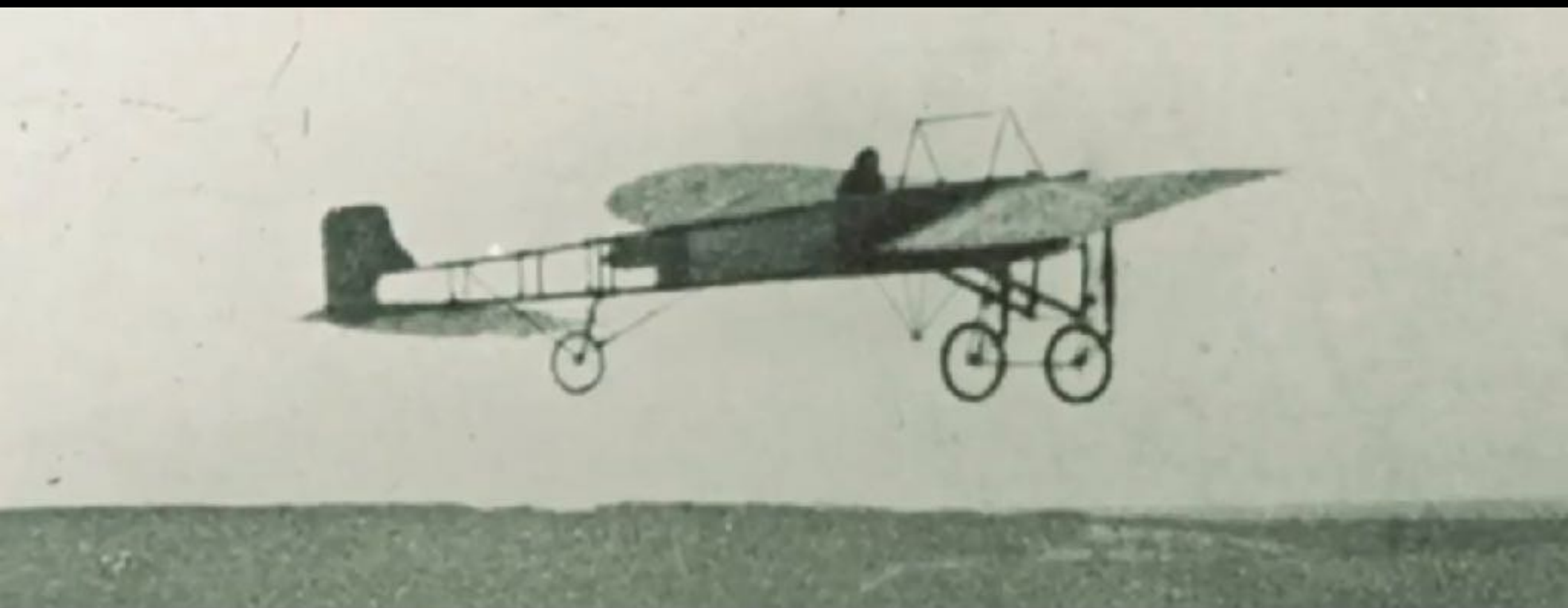






**Lindbergh
Bleriot
Herrick**





Louis Blériot makes the first powered flight across the English Channel



**Louis Bleriot
(1872-1936)**

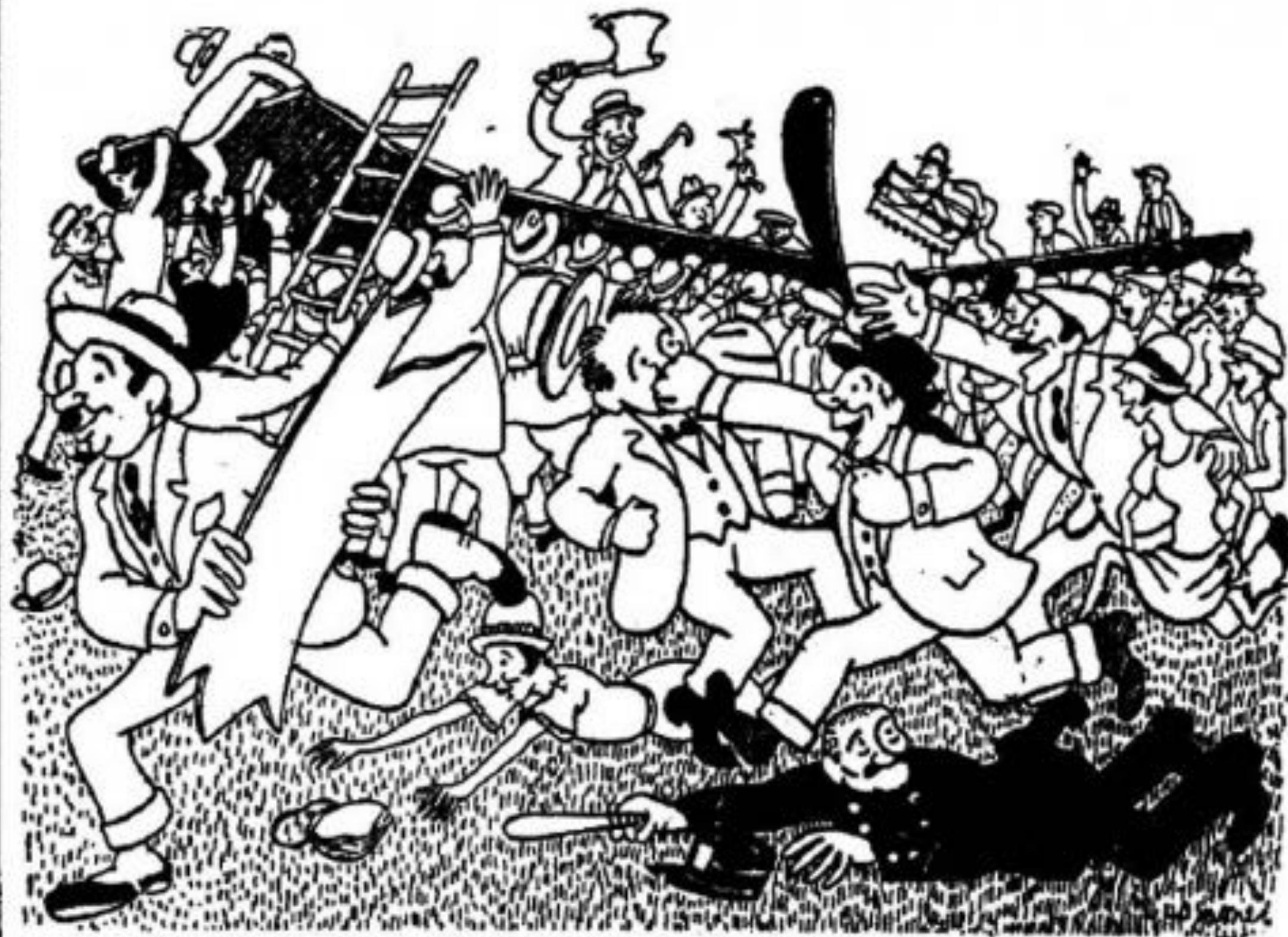
✈✈ HISTOIRE DE L'AVIATION ✈✈



REPUBLIQUE DES COMORES

75F





LINDBERGH LANDING AT PARIS

"Let us smash everything! Let us carry everything away for souvenirs! Let us show him what love is!"

—Cyrano (Paris).



MRS. LINDBERGH'S MESSAGE TOUCHES LOST ACE'S MOTHER

**Overjoyed to Receive Detroit
Woman's Son, So Much
Like Her Own Boy.**

BY HENRY WALES.

By Cable to Free Press and Chicago Tribune
Paris, May 24.—I carried a mes-
sage from Mrs. Evangeline L. Lind-
bergh, of Detroit, to old Mme. Nun-
gesser today.

Captain Lindbergh's mother,
reading the account of the touch-
ing meeting between the heart-
broken Frenchwoman and the boy,
who did what Charles Nungesser



FRANCE EXTOLS LINDBERGH'S EDUCATION

PRaise FOR LINDBERGH runs like a silver thread through the entire French press of the present time. A typical comment is that of *Le Progrès Civique*, of Paris, whose editor recalls a certain cartoon showing an airplane poised high above the sea. "Is it a Frenchman, an Englishman, or an American?" one spectator is asking. And the other replies: "It is a man." Taking this as his text, the editor continues: "Glory to Lindbergh, who has made us proud of being men! The whole world has felt his greatness. For the notable thing is the unanimity of the welcome given to this youth. Truly, not one false note. An admiration without reticence, without jealousy, without vulgar nationalism!"



The American system of education which helped to shape this heroic youth's character is the theme of an article by Raymond Gerard in *L'Echo des Sports*, of Paris, in which he says:

"The marvelous exploit achieved by Charles Lindbergh was due to exceptional qualities of courage and cool judgment. But one can say also that it is a product of the advantages of American education. Lindbergh, in physique and in morale, is a representative type of the younger generation of Americans."

"There is a world of difference between the shaping of minds in France and the preparation for life in America. French education is an affair of classes, of lessons, of studies, during which we pitchfork into the mind of the student the innumerable matters of school curriculum. The brain of a French high-school pupil is like a steamer trunk into which one packs a lot of widely different articles without regard to the destination of the tourist."

"American education is not at all like that. Before setting a big pile of books before the pupil, the teacher asks himself: 'Where are we going? What is the ideal to be attained?' The Greeks sought happiness through beauty. Modern nations seek to dominate by force. All right! Force is not obtained through books. For one sage we have ten men of action. Science can help, but the source of energy lies in character."

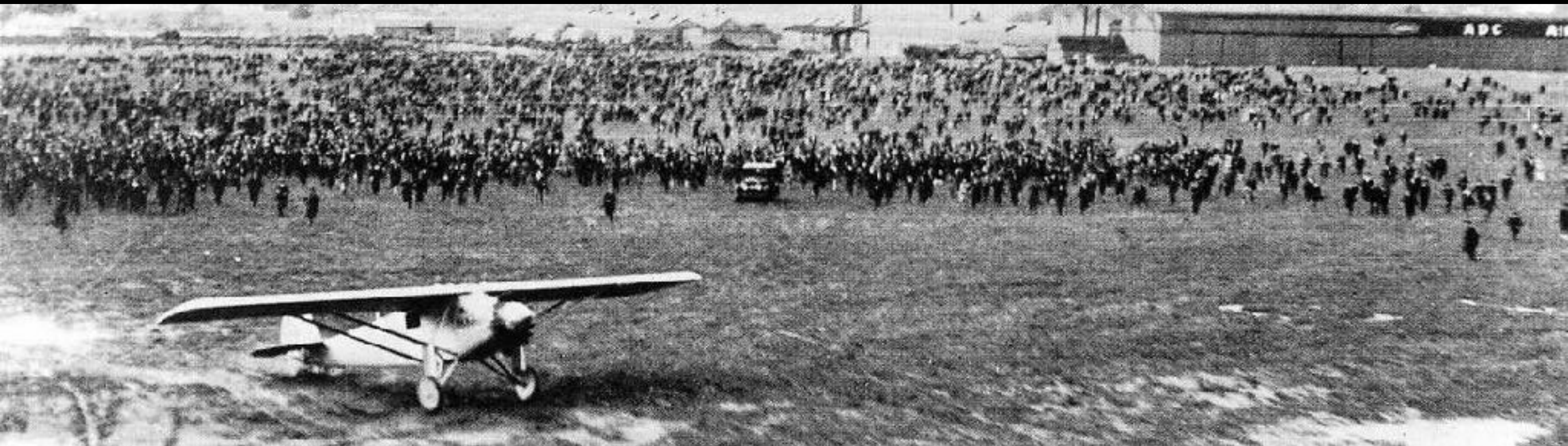
Let a nation forge souls, says M. Gérard, and success shall be its reward. He holds that the four bases of ideal character are vitality, courage, sensibility, and intelligence, and after defining the first two, he continues:

"Sensibility tempers the impetuosity of courage, it gives to a man the nobility of heart which surpasses all other virtues. Think of Lindbergh, and of how, a few minutes after his fantastic





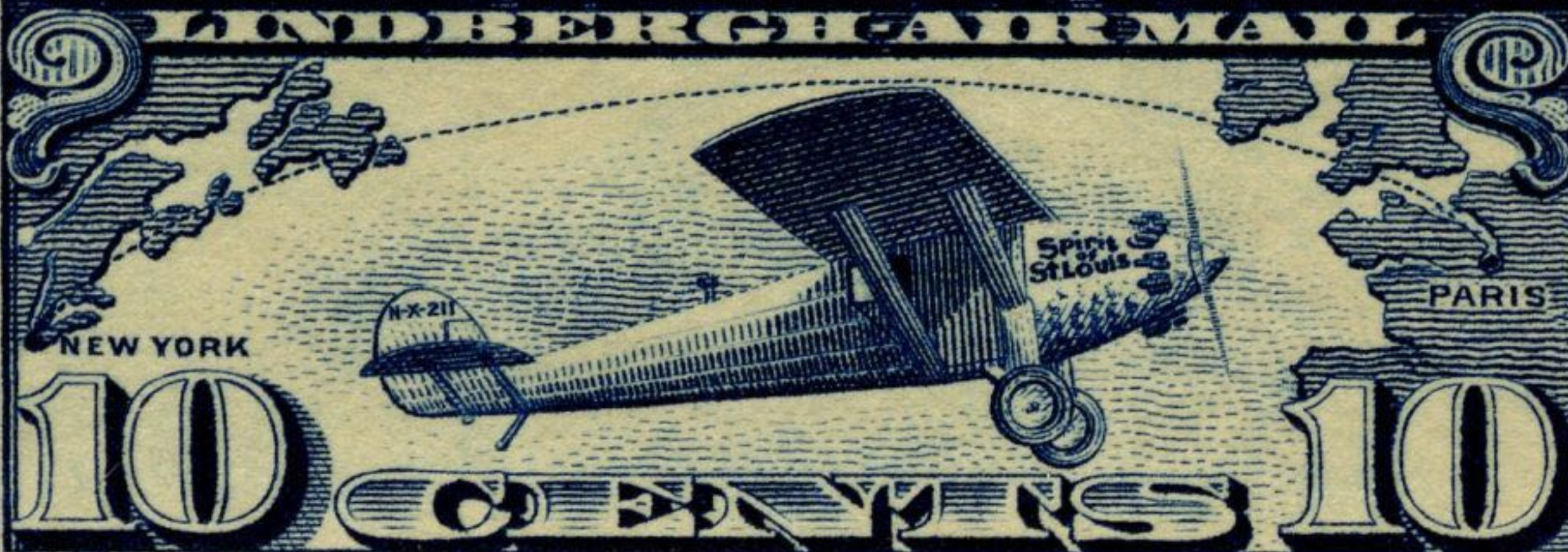
Herrick and Lindbergh waving the flag in Paris



Lindbergh lands in Britain



UNITED STATES POSTAGE
LINDBERGH AIR MAIL





AVIATION

The Oldest American Aeronautical Magazine

JUNE 20, 1927

Issued Weekly

PRICE 15 CENTS



President Coolidge Decorating Colonel Lindbergh with the Distinguished Flying Cross.

International News.

VOLUME
XXII

SPECIAL FEATURES

NUMBER
25

THE PIONEER EARTH INDUCTOR COMPASS
HOW THE NEW YORK TO PARIS PLANE WAS BUILT
WRIGHT WHIRLWIND A RESULT OF SEVEN YEARS' DEVELOPMENT

AVIATION PUBLISHING CORPORATION

Editorial and Business Office
250 WEST 57 STREET, NEW YORK

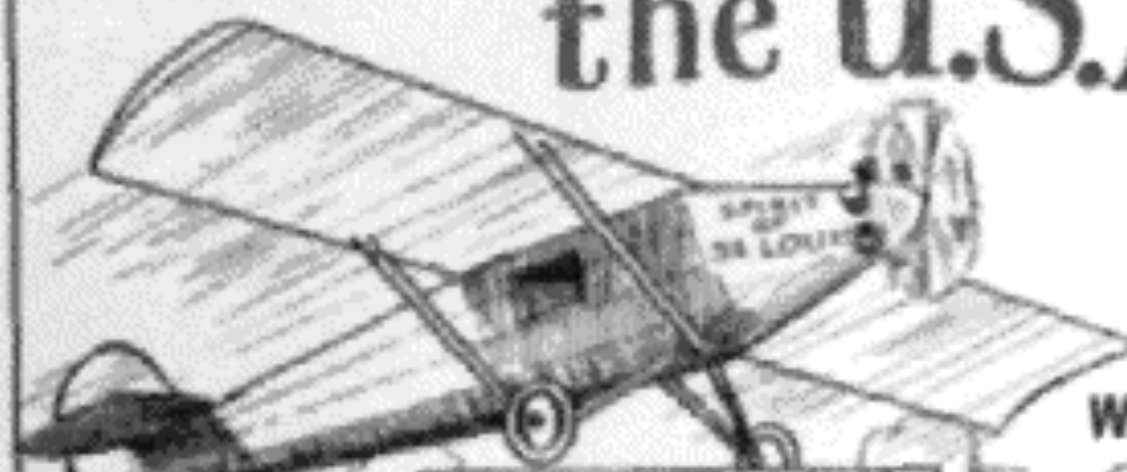
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under Act of March 3, 1879

*"Only
at the Alhambra"*

HEATHE ORGANIST
"MONTY"

He Belongs to the U.S.A.



WORDS
& MUSIC
BY
W.H. FRANK

To His
Devoted Mother
and to
Colonel Charles A. Lindbergh
the intrepid Hero of the Air
a song patriotically and gratefully
DEDICATED
in commemoration of those
ecstatic moments of greeting
upon Colonel Lindbergh's return
to America from
his triumphal flight
New York to Paris

As First song before the Lion's Club
by **MISS MARGARET E. GERBERICH**
Nationally Popular Soprano

© 1927 BY W.H. FRANK

The Official Spirit of St. Louis Welcome Song

"HELLO LINDY"



By
Larry Conley
AND
Dave Silverman

~ Dedicated To ~
CAPT. CHAS. A. LINDBERGH
AMERICA'S FLYING HERO
AS A TRIBUTE TO HIS SUCCESSFUL
TRANS-ATLANTIC NON-STOP FLIGHT
NEW YORK TO PARIS
MAY 20-21, 1927

*It is indeed a
pleasure to endorse
this Marvellous
Song*

*Sincerely
Victor J. Klemm
Mayor
City of St. Louis*

Published By
LARRY CONLEY INC.
MUSIC PUBLISHERS
SIXTHENT FIFTH THEATRE BLDG - ST. LOUIS, MO





Photo by The New York Times



**He was youth's answer to the wail of those
Who mourn a world swift crumbling to decay.**

**He was the hope that every woman knows
But some may never say.**

**The son who died, or was never born,
Who might have been like this. He was the bright
Indomitable breaking of the morn
After a weary night.**

**Look on this hero risen
Up from our earthly star,
And know that men need hardly be
Less than the angels are.**

**He is the poet of the air. He writes
In verse immortal that all men may read.**

**His meter is a motor-measured beat;
His thoughts aspire to clouds to distant heights.
He spells in piston-power of strength and speed.
His is the courage that defeats defeat.**

**A faithless generation asked a sign,
Some fresh and flaming proof of human worth,
Since youth could find no flavor in life's wine,
And there were no more giants in the earth.**

**Then out of gray obscurity he came
To laugh at space and thrust aside its bars;**

To manifest the littleness of fame

To one who has companioned with the stars.

**The drought of greed is broken,—fruitful streams
Of courage flow through fields long parched and dead;**

Young men see visions now, old men dream dreams,

A world moves forward with uplifted head:

**A lad with wings to dare had faith to rise
And carve proud arcs across uncharted skies.**

**Lithe stripling of the stock of pioneers,
You are that dream-self we all long to be,
Adventuring with purpose high and free,
Untouched by dross, untroubled by base fears.
Lead on! America's great future nears.
You are our pledge to all posterity,
A promise true, cloth'd in simplicity,
That we for history's finest can breed peers.**

**Blonde young Viking, flying, flying,
Like a sword that breaks the blue,
While the world remembers the men who made it,
It shall remember you.**



**You symbolize our splendid, secret dreams;
Ideals of manhood, virtues we hold dear.**





LIFE



© USPS 1998



Ruth Snyder murder trial

Average net paid circulation
of THE NEWS, Dec., 1927:
Sunday, 1,357,556
Daily, 1,193,297

DAILY NEWS
NEW YORK'S
PICTURE NEWSPAPER

**EXTRA
EDITION**

Vol. 9, No. 173 98 Pages

New York, Friday, January 18, 1928

2 Cents 18,000 4,000,000

DEAD!

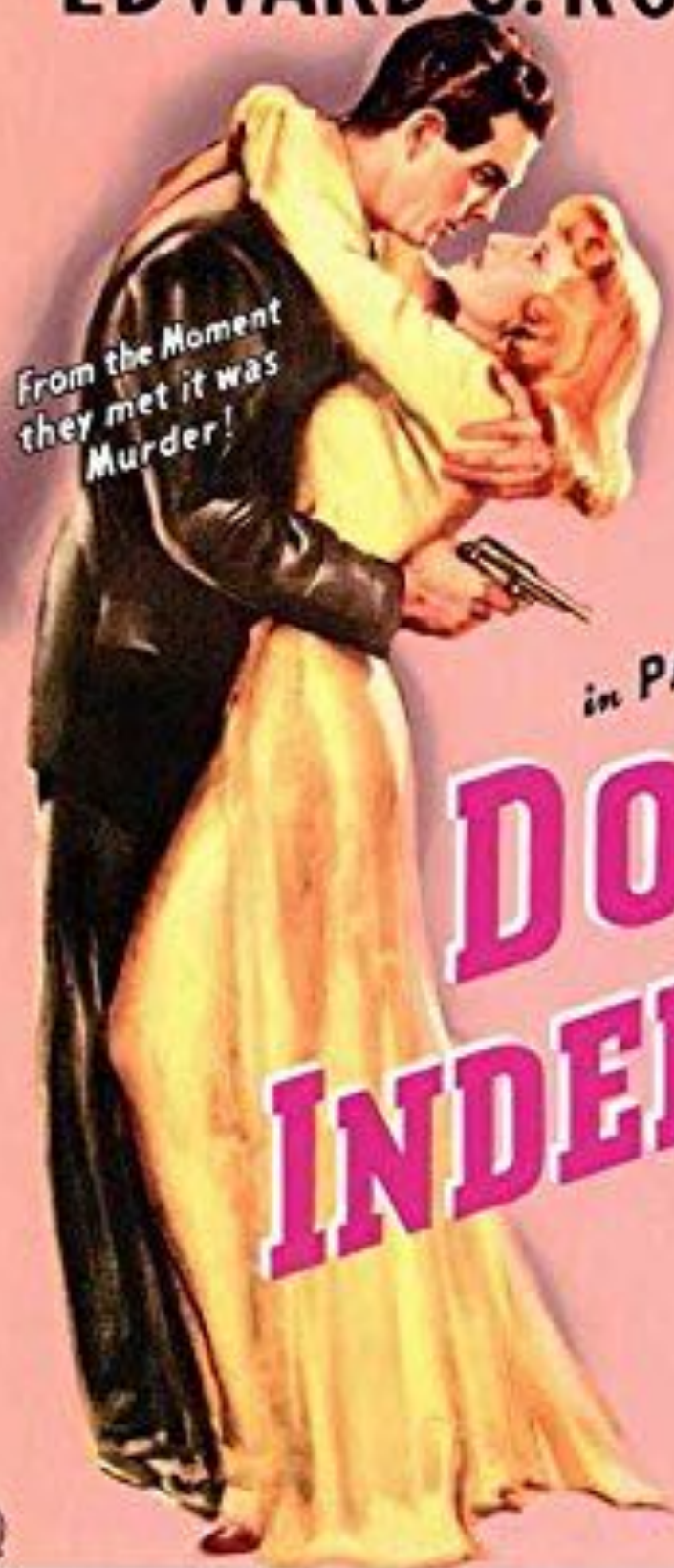
Story on page 3



(Copyright, 1928, by Pacific and Atlantic photos)

RUTH SNYDER'S DEATH PICTURED!—This is perhaps the most remarkable exclusive picture in the history of criminology. It shows the actual scene in the Sing Sing death house as the lethal current surged through Ruth Snyder's body at 11:06 last night. Her helmeted head is stiffened in death, her face masked and an electrode strapped to her bare right leg. The autopsy table on which her body was removed is beside her. Judd Gray, mumbling a prayer, followed her down the narrow corridor at 11:14. "Father, forgive them, for they don't know what they are doing?" were Ruth's last words. The picture is the first Sing Sing execution picture and the first of a woman's electrocution. Story p. 3; other pics. p. 28 and back page.

FRED
MACMURRAY ★ **BARBARA**
STANWYCK
EDWARD G. ROBINSON



in **PARAMOUNT'S**
DOUBLE
INDEMNITY

PORTER HALL
JEAN HEATHER
BYRON BARR
RICHARD GAINES
JOHN PHILLIBER

DIRECTED BY
BILLY WILDER

Screenplay by Billy Wilder and Raymond Chandler



No motion picture has ever come so close to a man, and an era...an era that belonged to young people. This is the most deeply moving personal adventure of our time!



JAMES STEWART
in his role of roles as Charles A. Lindbergh

the Spirit
of
St. Louis

BASED ON THE PULITZER PRIZE WINNING BOOK BY **CHARLES A. LINDBERGH**

...as the world held its breath...

CINEMASCOPE and **WARNERCOLOR** SCREEN PLAY BY **BILLY WILDER** and **WENDELL MAYES**
PRODUCED BY **LELAND HAYWARD** DIRECTED BY **BILLY WILDER** PRESENTED BY **WARNER BROS.**



Billy Wilder





LINDBERGH'S PARIS TRIP MAKES HIM WORLD HERO

FLOOD COVERS ONE SEVENTH OF LOUISIANA

New Orleans, La., May 21.—(AP)—The restless gurgles of muddy waters oozed from the northern boundary of Louisiana tonight to within fifty miles of the Gulf of Mexico after having cut a path 150 miles long and fifty miles wide across the state.

One seventh of the total area of the state was under water already and the flood was striking threateningly at weak points along the Atchafalaya, 140 miles northwest of New Orleans on the opposite side of the Mississippi, endangering five parishes with a total acreage amounting to almost half as much as already has felt the weight of the waters.

A stubborn fight still was being maintained at McCrea, on the Atchafalaya, where the fierce current was spilling embankments to pieces. More than 8000 workers were fighting desperately in the mud and rain to hold the flood waters off of fertile sugar plantations of Pointe a la Poudre, Assumption, Iberville, West Feliciana and Terrebonne parishes.

Situation Critical
With the situation critical along the eastern levee line of the Atchafalaya, the evacuation of the Evangeline country was proceeding rapidly. The population of the congregation camp at Lafayette already had sprung to 33,000 passing the total number of inhabitants of the city of New Orleans.

The flood waters had reached a point more than 75 miles south of the bridge at Bayou des Glaises and Melville and a solid sheet of water was spread over the fertile lands of the Evangeline country from the high lands in the vicinity of Bayou Bridge for 20 miles eastward to the levees of the Atchafalaya.

When the Teche signs of unyielding were being manifested, the muddy waters adding fury to their usually placid stream and the common warnings they were receiving were beginning to shake the firm belief that the flood would not reach that section. Two hundred and fifty people already had evacuated Lakeville, far down the Teche.

Boats still were pushing through the flooded territory, removing per-

LINDBERGH'S SUCCESS IS STORY OF UNSWERVING DETERMINATION

YOUTHFUL FLIER'S IDEALS ARE SAME AS ANY OTHER BOY'S

New York, May 21.—(AP)—The life story of Charles Lindbergh is a record of the attainment of a high ideal held to with unswerving determination.

At heart the gagging aviator is "just the boy he looks," says his friends, and his ideal, though not always the same, is one not dissimilar to that held by virtually every American boy. He had a "hobby" and he hoped to achieve greatness in his chosen field.

Born in the middle west in 1905, the son of Charles A. Lindbergh, who later became a congressman, Charles Lindbergh jumped at first destined for the life of a farmer. Up until six years ago he was plodding industriously behind plow and harrow, but every spare minute he spent on his old motorcycle or in any car that he could lay his hands on.

Besides that—and this has a bearing on the present flight for those who have feared the flier would be loquacious—he almost always drove alone. He liked solitude, and he likes it still.

On leaving farm work Lindbergh went to college, to study mechanical engineering, but according to fellow students he was not an academic success. He did more experimenting than most, they have said, but as soon as the results were plain to him he would go on to other work, not even bothering to turn in a report. So most of the marks he received were below passing.

Wanted to Be Aviator
Then one day an airplane landed on the college campus and a new fire was lit in this aloof boy's heart.

"I'm going to be an aviator," he told his mother without preamble and forthwith signed up for instruction at an aviation school.

From the moment he first curled his fingers around a control stick he was a success at flying. When he was through at the commercial school Lindbergh became a student



Charles A. (Left) Lindbergh, and the \$7,000 Monoplane in

YOUTHFUL AIRMAN WILDLY ACCLAIMED AFTER SPANNING A ATLANTIC IN NON-STOP DASH

A new epoch in aviation has been inaugurated.

Charlie Lindbergh, of Little Falls, Minn., landed at Le Bourget, France, at 5:15 p. m. eastern daylight time, yesterday, in one record-smashing jump from Roosevelt Field, New York.

"Well, here we are" was his greeting to the enthusiasm-maddened crowds.

Unaccompanied, Lindbergh drove his plane, "The Spirit of St. Louis," over the nearly four thousand mile air track, clipping about two hours and a half off the most optimistic time allowance.

The world's imagination was fired by his exploit.

Spontaneous celebrations in scores of cities both here and abroad lasted far into the night; President Coolidge and executives of other nations flashed their congratulations and these were supplemented by the thousand from other individuals publicly prominent.

At Detroit, Charles' mother relaxed her steadily maintained attitude of silent confidence and through tears of joy declared his victory, "was all that mattered."

CAPITAL THRILLED OVER LINDBERGH'S SUCCESSFUL FLIGHT

Washington, May 21.—(AP)—Lindbergh's history-making flight to France today thrilled Washington out of its traditional stolidity.

The thought of this young airman winging his way alone through the darkness of the night over desolate ocean waters, and depending for his life on his own skill and courage, aroused the interest and touched the imagination of the capital as few things have done in the past.

There has been the army's world circling flight, the boy of the N. C. 4 across the Atlantic, the trans-Atlantic voyage of the Los Angeles, and other feats of aviation which had brought admiration and praise from

Some Statistics On Lindbergh's Flight

A few statistics on the remarkable achievement of the Prince of America, Captain Charles A. Lindbergh, who is today receiving the accolade of the world:

To truly appreciate Captain Lindbergh's accomplishment, one must go behind the great and unprecedented "hop" from Roosevelt Field, N. Y., to Le Bourget field, Paris, France. It begins at San Diego, Cal., where the intrepid flier really started his world flight, thence to St. Louis, Mo., a distance of 1,800 miles and thence to New York, a distance of 850 miles. The total distance of the continental flight of 2,650 miles was made in 31 hours flying time.

Then came the great "hop" from New York to Paris, a distance of 3,600 miles, 1,900 of which was over open seas, in 33 1/2 hours.

Paris, May 21.—(By the Associated Press.)—Captain Charles A. Lindbergh, the young American aviator, who hopped off from New York yesterday morning all alone in his monoplane, arrived in Paris tonight, safe and sound as everyone hoped he would.

The sandy haired son of the middle west dropped down out of the darkness at Le Bourget Flying Field, a few miles from Paris, at 10:21 o'clock tonight, (5:21 P. M. New York time.) only 33 1/2 hours after leaving Long Island—the first man in history to go from New York to Paris without changing his seat.

To the young American it was seemingly merely the achievement of an ambition. To Paris, to France, to America, to the world, his landing tonight made him the greatest of heroes mankind had produced since the air became a means of travel.

A crowd of at least 25,000 surrounded his plane, the "Spirit of St. Louis," when it came to earth after its epochal voyage from the new world to the old.





"WE"

BANQUET
in honor of

CAPTAIN CHARLES A. LINDBERGH

"AMERICA'S AIR AMBASSADOR"

NEW YORK *to* PARIS NON-STOP FLIGHT



